



Comhairle Chontae na Gaillimhe
Galway County Council

GALWAY COUNTY COUNCIL

CLIFDEN TOWN PUBLIC REALM ENHANCEMENT PROJECT INCLUDING
CLIFDEN HARBOUR PARK AND BEACH ROAD QUAY CLIFDEN

STAGE 1 ROAD SAFETY AUDIT



Clifden Town Public Realm Enhancement Project Including Clifden Harbour Park and Beach Road Quay Clifden

STAGE 1 ROAD SAFETY AUDIT

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1.0 INTRODUCTION

This report describes the Stage 1 Road Safety Audit carried out for proposed active travel upgrade works in Clifden Town.

This report covers the following schemes:

- Clifden Town Public Realm Enhancement
- Harbour Park
- Beach Road

The scheme locations are shown in Figure 1-1.



Figure 1-1 Scheme Location Map

1.1 EXISTING ENVIRONMENT

1.1.1 Clifden Town Public Realm Enhancement

Clifden Town is situated adjacent to the N59, National Road but does not include works on the National Road. The proposed site for audit includes urban and suburban areas within Clifden Town Centre

The main street runs through the heart of the village. The schemes are located within the urban 50kph speed limit, commencing at N59 Clifden Business Park and continuing towards Clifden village.

The main street is a single carriageway road with pedestrian facilities currently provided on both sides of the road. There are no cycle facilities present. Ribbon development is along both sides of the road.

1.1.2 Harbour Park

Harbour Park is an area of vacant land at the water edge sited beside the town's playground and basketball court.

1.1.3 Beach Road

The pier is located on the Beach Road on the northern side of Clifden Bay. Clifden bay is located to the west of Clifden town centre.

There is a grass verge approximately 3m wide between the top of the pier and the Beach Road over most of the length of the wall with the exception of one area where there is a concrete pavement adjacent to the wall.

The pier seems to be used mainly by small leisure craft and fishing vessel used by fishermen.

1.2 PROPOSED SCHEME

Galway County Council propose to upgrade sections of the urban Clifden Town area, Harbour Park and Beach Road Quay with public realm, pedestrian and cycleway upgrades.

A summary of the proposed works for Clifden Town Centre are as follows:

- Junction Upgrades
- Urban Realm Upgrades
- Car Park Upgrades
- Streetscape amendments

A summary of the proposed works for Harbour Park are as follows:

- Car Park Upgrades
- Boundary Treatment

A summary of the proposed works for Beach Road Quay are as follows:

- Car Park Upgrades
- Integration of signage and enhanced connectivity

1.3 COLLISION DATA

1.3.1 Road Safety Authority Database

Collision data has not been supplied with this scheme.

Road Collision Data is not currently available on the Road Safety Authority Database, and therefore the audit team has no access to the historical collision information for this site and / or adjacent roads.

1.4 AUDIT DETAILS

The audit took place in TOBIN Offices in September 2023. The audit comprised an examination of the documents provided by the Design Team and listed in Appendix A. A site visit was undertaken by the RSA Team on the 12th of September 2023, between the hours of 9:30-12:00pm. During the site visit the weather was dry with some sunny spells and the road surface was dry.

The Audit Team note that no information was provided on the following:

- Vertical / Longitudinal profiles of the carriageway are existing and have not been assessed as part of this RSA. The active travel infrastructure has been assessed taking cognisance of the constraint onsite.

- Detail design elements – (Ancillary highway drawings including, amendments to existing utilities and public lighting, geometry, drainage, pedestrian and vehicular restraints, vehicular wept paths, site clearance, boundary treatments, earthworks and excavations, pavement details & specific kerbing details, accommodation works and as such the Audit team have not considered these elements in the Stage 1 Road Safety Audit Report.

The audit team members were as follows:

Audit Team Leader/Member

- Ronan Murtagh – B.A. B.A.I, CEng, MIEI. Senior Engineer for Roads & Transportation, TOBIN Consulting Engineers. – TII Reference RM3414512
- Maria Rooney B.Eng., M.Eng., C.Eng., MIEI Senior Engineer for Roads & Transportation, TOBIN Consulting Engineers. – TII Reference MR3384505

Audit Team Observers:

- Jessica Lima –BEng (Hons) Civil Eng., MIEI. Design Engineer for Roads & Transportation, TOBIN Consulting Engineers

This Stage 1 Audit has been carried out in accordance with the relevant sections of Transport Infrastructure Ireland Publication (Standards) “Road Safety Audit” GE-STY-01024 (December 2017). The team have examined and reported only on the road safety implications of the design submitted and has not examined or verified the compliance of the design to any other criteria. However, to clearly explain a problem or a recommendation, it may be necessary to refer to another Standard or Advice Note, but such reference will not conflict with the requirements of the above Terms of Reference.

The Design Team and Employer (Client) is reminded that the Road Safety Audit Feedback Form, in Appendix C, shall be completed and returned to the Road Safety Audit Team Leader for sign off.

2.0 ITEMS RESULTING FROM THIS ROAD SAFETY AUDIT

2.1 GENERAL ITEMS CLIFDEN TOWN CENTRE

2.1.1 PROBLEM

General Item – Tactile Paving

The Audit Team observed on site a number of tactile paving was absent at numerous proposed crossings. The Audit Team are concerned this may result in visually impaired personal will enter the roadway. This may result in a collision between a vulnerable road user (VRU) and a vehicle.

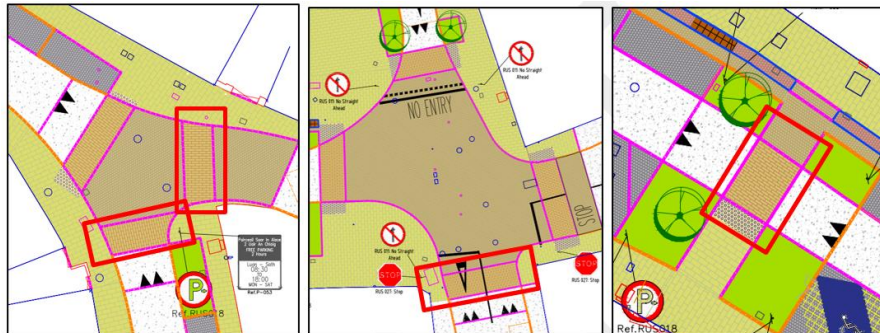


Figure 2-1 Examples of Discrepancy of Tactile Paving Provisions (Dwg No. 11623_LD_HL_TC_003)

Recommendation

The designer should provide tactile paving at all crossing points throughout the scheme.

2.1.2 PROBLEM

General Item – Road Marking

The Audit Team observed on the design that the proposed road markings are not in accordance with chapter 7 of the Traffic Signs Manual (TSM). The Audit Team are concerned that the Inaccurate Road marking selection and location could result in a false expectation of the upcoming traffic. This may result in collisions between vehicles or increased risk of collisions with VRU.

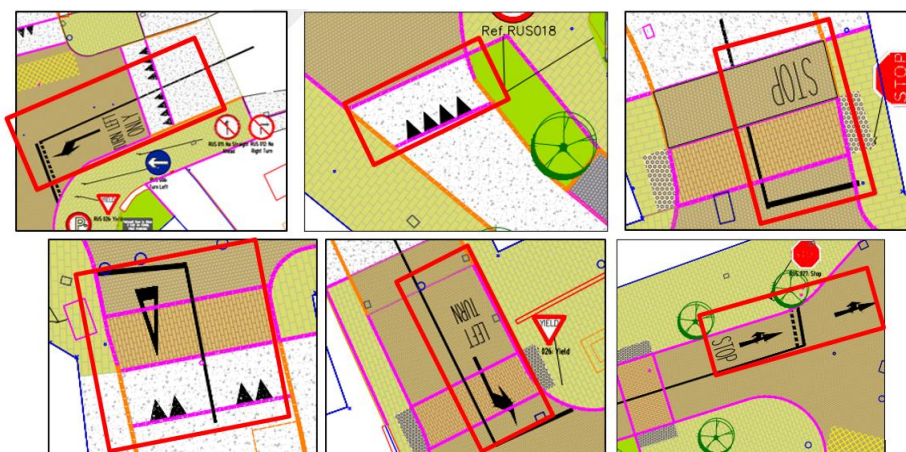


Figure 2-2 Examples of Discrepancy Road Markings (Dwg No. 11623_LD_HL_TC_002)

Recommendation

The Design Team should provide consistent delineation to Chapter 7 of the TSM across the scheme.

2.1.3 PROBLEM

General Item – Signage

The Audit Team observed on the design that the proposed signage design is not in accordance with the Traffic Signs Manual (TSM). The Audit Team are concerned that Inaccurate signage selection and location of signs could result in a false expectation of the upcoming road operation and layout. This may result in collisions between vehicles or increased risk of collisions with VRU in the proximity of the crossing location.

Recommendation

The Design Team should provide consistent delineation & signage in accordance with TSM across the scheme.

2.1.4 PROBLEM

General Item – Existing Utility

The Audit Team observed on site a number of clashes with existing utilities, poles interfacing kerb lines and obstructing footways. The Audit Team are concerned this may result in trip and point of hazards for VRU. This may result in an obstruction for VRU and vehicles.

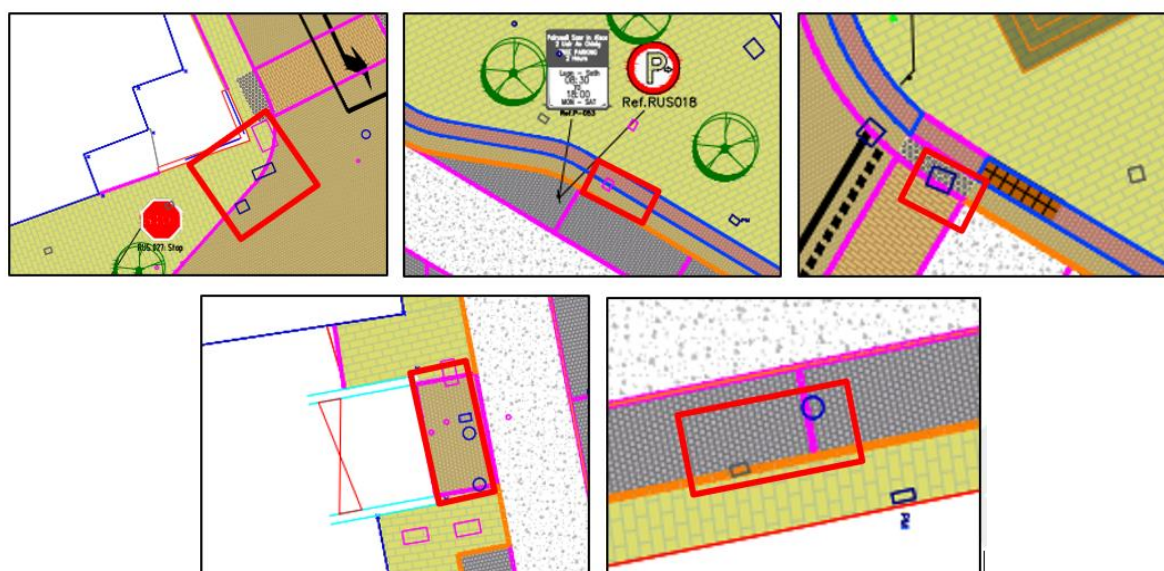


Figure 2-3 Examples of Clashes with Existing Utility Chambers and Proposed design (Dwg No. 11623_LD_HL_TC_003)

Recommendation

The designer should divert the existing utilities to remove obstructions for pedestrians and drivers or modify the design to avoid utility clashes.

2.1.5 PROBLEM

General Item - Footway / Cycle Track / Carriageway Widths

The Audit team observed on site that carriageway, footway and cycleway widths weren't shown on the drawings. The Audit Team is concerned that inadequate width facilities may result in opposing user collisions or entry into adjacent facility. This may result in a collision between a VRU and a vehicle.

Recommendation

The design team should ensure that all facilities are of suitable width and in compliance with the relevant design manuals.

2.1.6 PROBLEM

General Item - Displaced Local Traders

The Audit team observed on site a number of local traders are operating within parallel parking areas and offline parking areas on Market Street. The Audit team are concerned the failure to cater for the existing traders may result in the setting up of the stalls in unsuitable locations. This may result in creating hazardous situations for VRU.



Figure 2-4 Examples of Existing Local Traders in Market Street.

Recommendation

The designer should accommodate existing trader areas within the scheme.

2.1.7 Problem

General Item -Lack of Information at Tie in Locations

The Audit team observed on site that a proposed full upstand is shown on the drawings but on the drawings no information on tie in location has been provided. The Audit Team is concerned that an incomplete tie in design may result in drivers' confusion or entry into adjacent facility. This may result in a collision between a VRU and a vehicle.

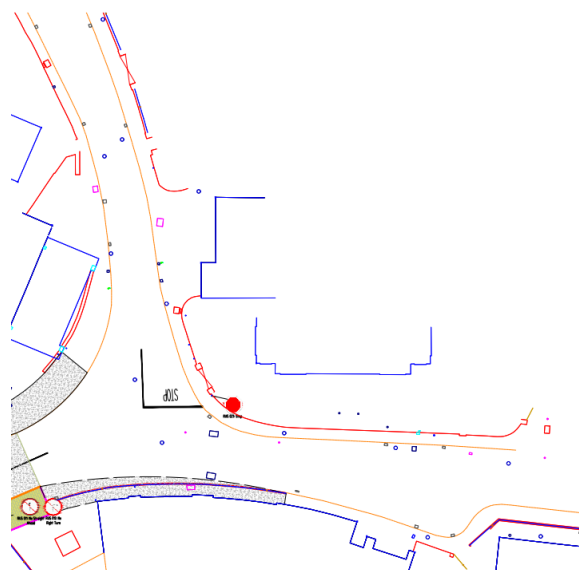


Figure 2-5 Example of Lack of Tie in Location provided (Dwg No. 11623_LD_HL_TC_002)

Recommendation

The design team should ensure that design indicate tie in location.

2.1.8 PROBLEM

Main Street/ Bridge Street/ Market Street Priority Operation

The Audit Team observed the preliminary design that no details of the operation of the junction have been provided to the Audit Team. The Audit Team are concerned that the lack of clarity on priority of road users could result in driver confusion and turning collisions. This may result in a collision between a VRU and a vehicle.

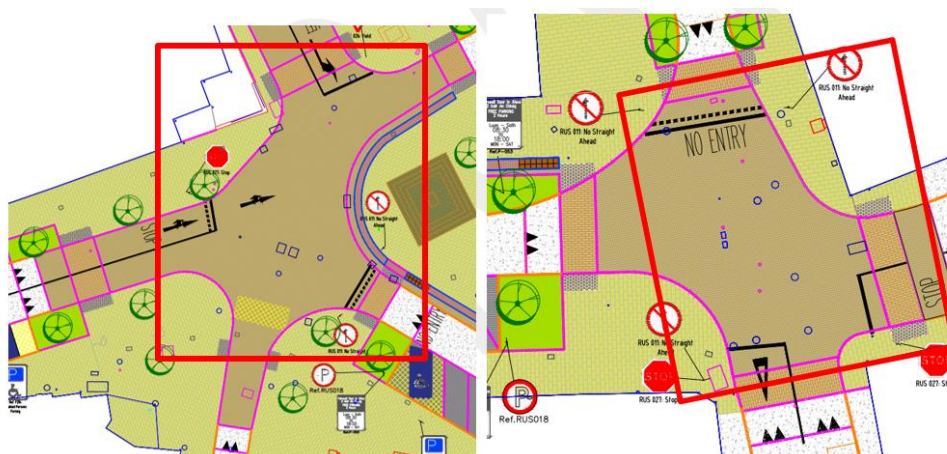


Figure 2-6 Example of no details of priority at junctions have been provided (Dwg No. 11623_LD_HL_TC_001 & 002)

Recommendation

The designer should show the priority at junctions within the urban area, with clearly defined road marking and signage.

2.1.9 PROBLEM

Pedestrian Priority

The Audit Team observed the preliminary design that no details of the footway continuity have been provided to the Audit Team. The Audit Team are concerned that the lack of clarity on priority of footway could result in VRU and driver confusion. This may result in a collision between a VRU and a vehicle.



Figure 2-7 Example of no details of priority at footway have been provided (Dwg No. 11623_LD_HL_TC_002)

Recommendation

The designer should show the continuity at footway within the urban area, with clearly defined road marking and signage.

2.1.10 PROBLEM

Obstructions Within Proposed Footway Areas

The Audit Team observed on site numerous areas where clashes occurred with existing urban realm infrastructure. The Audit Team are concerned this may result in obstruction for VRU. This may result in an obstruction for pedestrians leading to potential collisions and conflicts occurring.



Figure 2-8: Existing Obstructed Regulatory Sign

Recommendation

The designer should remove the existing urban realm infrastructure obstructions.

2.1.11 PROBLEM

Cycle Lane Termination

The Audit team noted that no transition detail has been provided at the termination of this cycle lane as part of the preliminary design. The Audit Team are concerned that this may give rise to a conflict point between pedestrians and cyclists. This may result in an obstruction for VRU and cyclists.

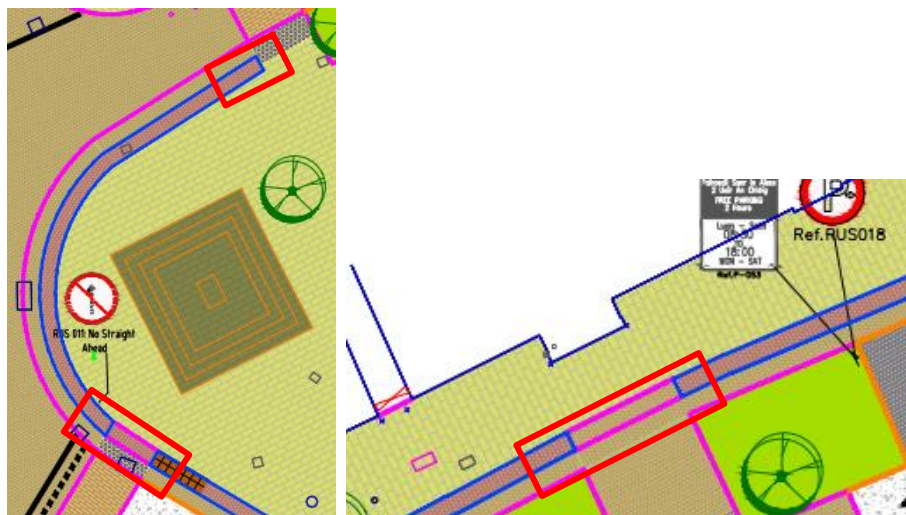


Figure 2-9: No Proposed Cycle Lane termination measure (Dwg No. 11623_LD_HL_TC_001)

Recommendation

The design team should ensure the cycle lane is terminated appropriately at the end point.

2.1.12 PROBLEM

Provision Of Paving at Steps Location

The Audit Team observed the preliminary design proposed paving works and does not include a treatment to provide warning to visually impaired road users. The Audit Team are concerned that this may result in a trip hazard for VRUS. This may result in collisions with visually impaired users and VRUs.

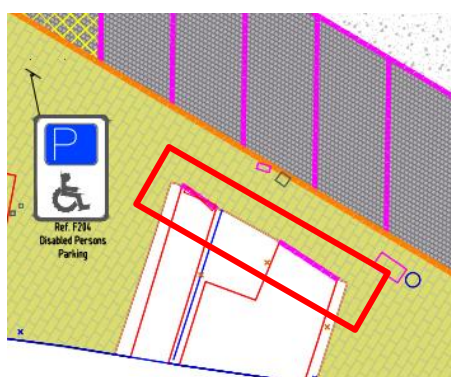


Figure 2-10: Example of Existing Steps (Dwg No. 11623_LD_HL_TC_001)

Recommendation

The design team should provide suitable gradients for pedestrians & VRUs across the scheme and ensure protective measures to prevent wheelchair rolling into the carriageway.

2.1.15 PROBLEM

Kerb Type Inconsistency

The Audit Team observed on site that parking facilities have not been provided for this location. The Audit Team are concerned the lack of parking can enforce drivers to park on the footway. This may result in an obstruction to the footway.

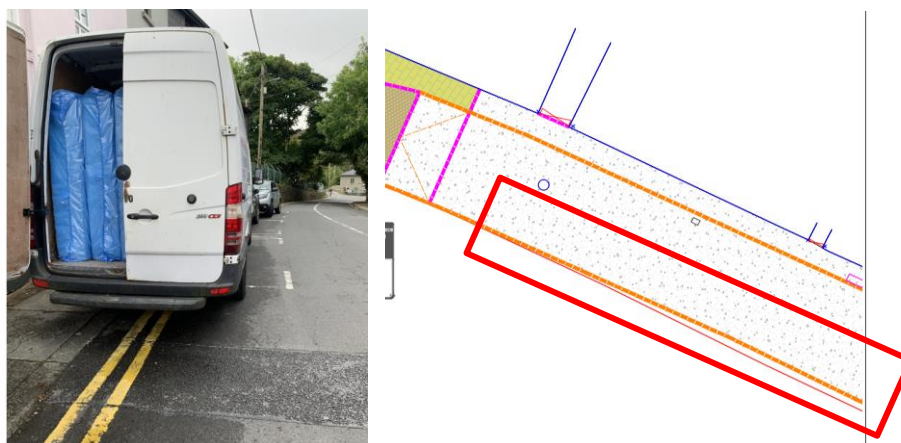


Figure 2-13: Existing Upstand Kerb and Parking (Dwg No. 11623_LD_HL_TC_003)

Recommendation

The Design team should account for existing kerb type and parking facilities.

2.1.16 PROBLEM

Proposed Road Sign Setback

No details were provided of the height and setback of the road signs from the kerb line. The audit team are concerned the road signs height and setback too close to the kerb line and the visibility of proposed signs. This will result in damage to the vehicle, the sign and potentially result in the sign being removed.

Recommendation

The design team should set back all road signs from the kerb line in accordance with the Traffic Signs Manual (TSM).

2.1.17 PROBLEM

Existing Loading Bay

The Audit team observed on site that the existing loading bay has not been incorporated into design. The audit team are concerned this may lead to parking two abreast when loading is required. This may lead to driver frustration, unsafe overtaking and head-on collisions.



Figure 2-14 Loading Bay

Recommendation

The design team should incorporate the existing loading bay into the design or provide alternative loading area.

2.1.18 PROBLEM

Pedestrian / Cyclists Visibility

The Audit team observed on the design that the proposed trees is shown on both side of the footway and cycleway. The Audit are concerned that cyclists or pedestrians may proceed into the path of oncoming traffic. This may result in a collision between VRU and a vehicle.



Figure 2-15 Pedestrian and Cyclists Visibility (Dwg No. 11623_LD_HL_TC_003)

Recommendation

The design team should check the visibility approaching the ramp.

2.1.19 PROBLEM

Road Marking

The audit team noted 'No Entry' road markings is proposed with the wrong orientation. The audit team are concerned this may lead to drivers' confusion. This may result in collisions between vehicles or increased risk of collisions with VRU.



Figure 2-16 No Entry Road marking (Dwg No. 11623_LD_HL_TC_003)

Recommendation

The design team should ensure road markings are in compliance with the TSM.

2.1.20 PROBLEM

Road Marking

The Audit team observed that at the Junction coming from Bridge Street, a left turn road markings has not been proposed. The audit team are concerned this may lead to drivers' confusion and late turning movement without warning. This may result in a collision between VRU and a vehicle.



Figure 2-17 Left turn road marking not included on the design (Dwg No. 11623_LD_HL_TC_001)

Recommendation

The design team should ensure road markings are in compliance with the TSM.

2.1.21 PROBLEM

Gradient Of Angled Car Parking Bays

The Audit team observed angled car parking bays are proposed in the area of Market Street and Main Street. The existing spaces are angled such that easy entry by driving forward can be achieved with a wide carriageway to reverse out from. The Audit Team are concerned that the driver reversing into spaces with excessive gradients could result in a loss of control and lead to collisions with VRUs.

Recommendation

The design team should ensure suitable gradients of the angled parking bays at detailed design

2.1.22 PROBLEM

Road Marking at Crossing

The Audit team observed that double yellow lines are not proposed on approach to all crossings. The Audit are concerned that vehicles parked on approach to the crossing may restrict visibility to pedestrians who are commencing their crossing. This may result in a collision between VRU and a vehicle.

Recommendation

The design team should ensure double yellow lines on approach to all junctions.

2.1.23 PROBLEM

Bus Bay Road Marking and Taper

The Audit team observed on site that no detail on road marking and taper lengths have been provided for the bus bay. The Audit are concerned that the bus will restrict visibility to/from pedestrians who are using the adjacent crossings. This may result in a collision between VRU and a vehicle.

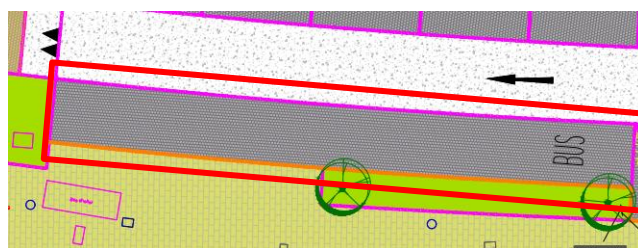


Figure 2-18 Bus Bay Road marking and taper (Dwg No. 11623_LD_HL_TC_001)

Recommendation

The design team should provide bus bay markings in accordance with the Traffic Signs Manual (TSM).

2.1.24 PROBLEM

Existing Bollards

The Audit team observed on site that during the site visit that no details has been provided on the existing bollards in front of Circle K at Main Street.



Figure 2-19 Existing Bollards

Recommendation

The audit team should reinstate the existing bollards.

3.0 OBSERVATIONS

The following observations were noted by the Road Safety Audit team on site and have been included below to raise some potential design considerations on this scheme.

3.1 OBSERVATION 1

The following elements have not been provided to the audit team for the Stage 1 Road Safety Audit.

- geometry,
- drainage
- utilities
- public lighting and accommodation work.
- A significant lack of consistency was also noted in the design elements provided including paving, tactile at crossings, kerbs, footways and paved areas.

The audit team note that the significant lack of detail provided for this Stage 1 RSA may give rise to follow on safety problems for the Stage 2 RSA which the design team should be conscious of prior to commencing detailed design.

3.2 OBSERVATION 2

Stop Line Road Marking

The Audit team observed during the site visit that the visibility of the stop line road marking overshoots the stop line. The Audit are concerned that vehicles at the stop line won't see the upcoming traffic from Main Street. This may result in a collision between vehicles.



Figure 3-1 Stop Line Road Marking

Recommendation

The Design team should check the visibility of the junction.

3.3 OBSERVATION 2

Existing Drainage

The Audit team observed during the site visit that the Aco drain has not been included in the proposed design. This can lead to future drainage issue.

Recommendation

The design team should ensure that ECO drain is included in the design.



Figure 3-2 Existing Aco Drain

4.0 ITEMS RESULTING FROM THIS ROAD SAFETY AUDIT

4.1 GENERAL ITEMS HABOUR

4.1.1 *PROBLEM*

The following elements have not been provided to the audit team for the Stage 1 Road Safety Audit.

- sight lines,
- swept paths.
- drainage
- utilities
- accommodation work.

4.1.2 *PROBLEM*

General Item Visibility Splays

The Audit team observed during the site visit no visibility splays have been provided to the audit team. Lack of clarity on visibility for pedestrians and drivers could result in collisions occurring.

Recommendations

The designer should show adequate visibility splays at all pedestrian crossing, vehicular accesses and junctions in accordance with the relevant standards.

4.1.3 *PROBLEM*

Clash with Existing Public Lighting Pole

The Audit team observed during the site visit the existing public lighting pole has not been included as part of the proposed design. This Audit are concerned this may result in a clash with the proposed design.

Recommendation

The design team should show relocation of existing pole.

5.0 ITEMS RESULTING FROM THIS ROAD SAFETY AUDIT

5.1 GENERAL KEY ROAD

5.1.1 PROBLEM

The following elements have not been provided to the audit team for the Stage 1 Road Safety Audit.

- sight lines,
- swept paths.
- drainage
- utilities
- accommodation works.

5.1.2 PROBLEM

Boundary Treatment

The Audit team observed during the site visit that no boundary treatment has been provided to protect road users near the pier along an active docking area. This may result in vulnerable road users (inc. pedestrians, cyclists, scooter users, motorcyclists, less abled and mobility impaired users) falling into the water.



Figure 5-1 Boundary Treatment not proposed on design.

Recommendations

The designer should show adequate boundary treatment is provided.

6.0 AUDIT TEAM STATEMENT

We certify that we have examined the drawings and other information listed in Appendix A and visited the site during the day of the 9th of February 2023. We further certify that we are independent from the design team for the scheme. This examination has been carried out with the sole purpose of identifying any features of the design that could be removed or modified to improve the safety of the scheme. The problems that we have identified have been noted in the report, together with suggestions for improvement that in our opinion should be studied for implementation.

AUDIT TEAM LEADER

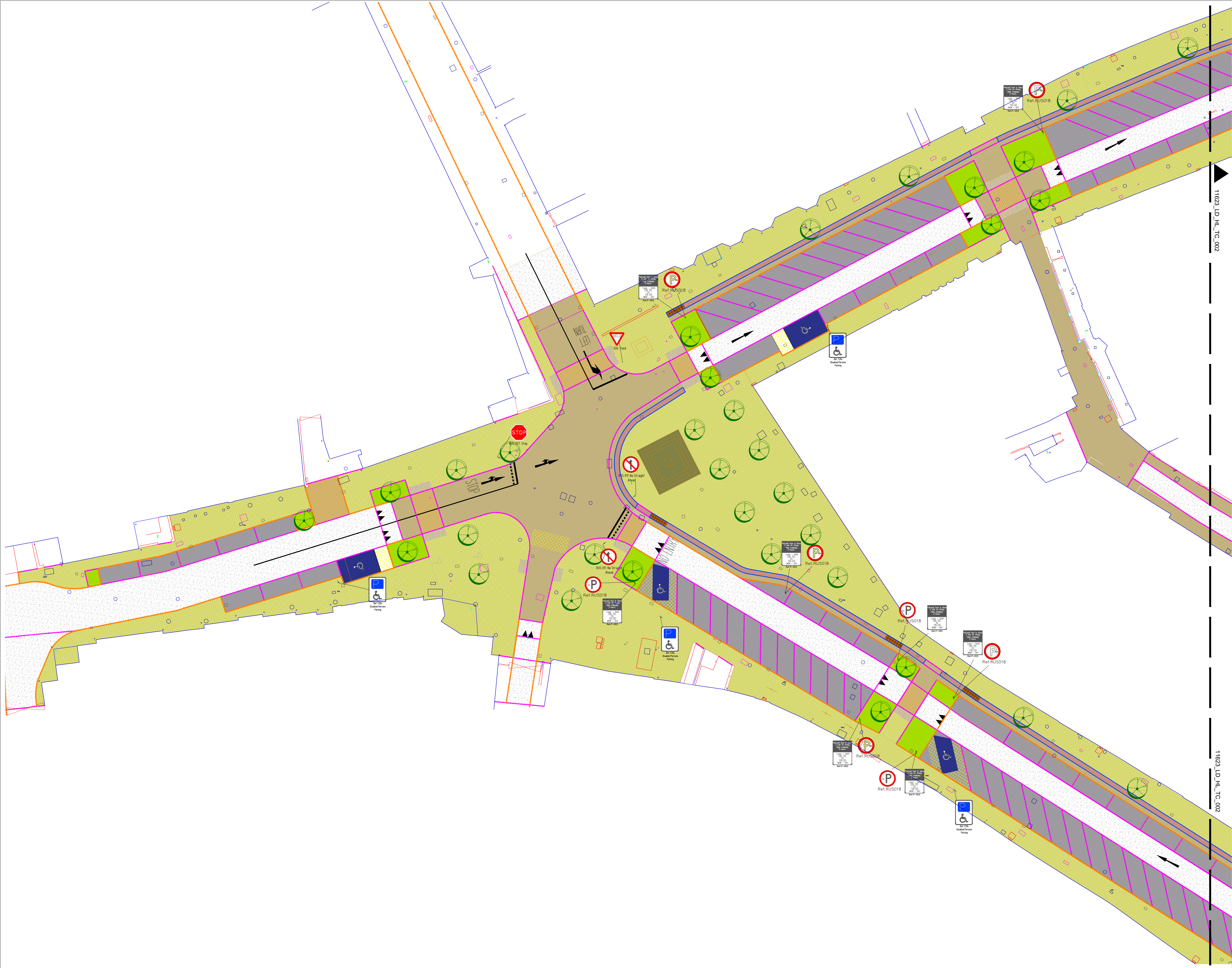
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AUDIT TEAM MEMBER

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Appendix A – List of Documents Examined

- Drawings
 - 11623_LD_HL_TC_001 Clifden Town Centre Public Realm
 - 11623_LD_HL_TC_002 Clifden Town Centre Public Realm
 - 11623_LD_HL_TC_003 Clifden Town Centre Public Realm
 - 11632-LD-PLN-003 Beach Road Quay General Arrangement
 - Harbour Park



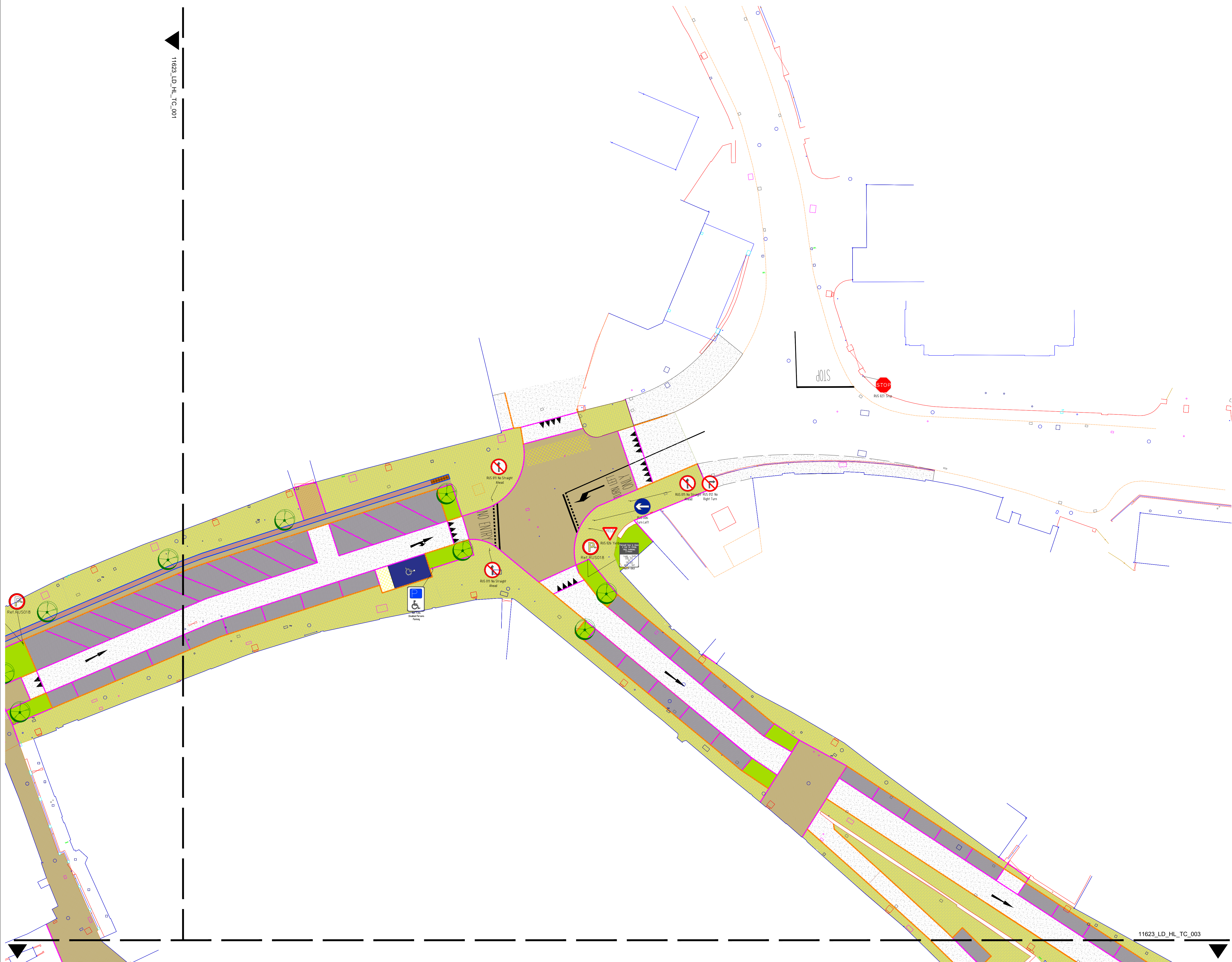
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- Proposed stone paving to footways
- Proposed sett paving shared surfaces
- Proposed sett paving to pedestrian crossing points
- Proposed sett paving to parking spaces
- Proposed asphalt surfacing to carriageways
- Proposed sett paving to contraflow cycleway
- Proposed tactile surfacing
- Proposed corduroy tactile paving to cycleway
- Proposed flush kerb (minimal upstand for drainage)
- Proposed kerb with upstand
- Proposed active travel kerb to cycleway
- Proposed planting
- Proposed street tree planting

Iss	Date	Description	Chk	Chk
P01	08.09.23	RSA Issue	CC	Ta

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manchester@landuse.co.uk
www.landuse.co.uk

Project Clifden Town Centre Enhancements	
Client Galway County Council	
Scale @A1 1:250	Status Information
Drawing Title Clifden Town Centre Public Realm (sheet 1 of 3)	
Job Nr 11623	Drawing Nr 11623_LD_HL_TC_001
Issue P01	



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- | | |
|---|--|
|  | Proposed stone paving to footways |
|  | Proposed sett paving shared surfaces |
|  | Proposed sett paving to pedestrian crossing points |
|  | Proposed sett paving to parking spaces |
|  | Proposed asphalt surfacing to carriageways |
|  | Proposed sett paving to contraflow cycleway |
|  | Proposed tactile surfacing |
|  | Proposed corduroy tactile paving to cycleway |
|  | Proposed flush kerb (mini upstand for drainage) |
|  | Proposed kerb with upstand |
|  | Proposed active travel kerb to cycleway |
|  | Proposed planting |
|  | Proposed street tree planting |

01	06.09.23	RSA Issue		CC	TJo
as	Date	Description		Dir	Chk

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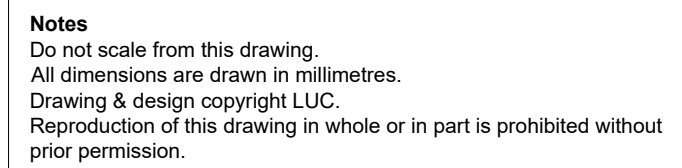
Project
Clifden Town Centre Enhancements

Client
Galway County Council

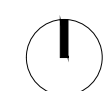
Scale @A1 1:250	Status Information
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Drawing Title
Clifden Town Centre Public Realm
(sheet 2 of 3)

Job Nr 11623	Drawing Nr 11623_LD_HL_TC_002	Issue P01
-----------------	----------------------------------	--------------



- | | |
|---|--|
|  | Proposed stone paving to footways |
|  | Proposed sett paving shared surfaces |
|  | Proposed sett paving to pedestrian crossing points |
|  | Proposed sett paving to parking spaces |
|  | Proposed asphalt surfacing to carriageways |
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|  | Proposed active travel kerb to cycleway |
|  | Proposed planting |
|  | Proposed street tree planting |



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Project	Clifden Town Centre Enhancements
---------	----------------------------------

Client	Galway County Council
--------	-----------------------

Scale @A1

Scale @A1 1:250	Status For Information
--------------------	---------------------------

Drawing Title
Clifden Town Centre Public Realm
(sheet 3 of 3)

Job Nr 11623	Drawing Nr 11623_LD_HL_TC_003	Issue P01
-----------------	----------------------------------	--------------

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Key

Existing and Retained

-  Existing asphalt
-  Existing quay wall
-  Existing sandstone paving
-  Existing feature paving
-  Existing handrail
-  Existing telescope
-  Existing utilities

Proposed

-  Stone paving
-  Stone feature paving
-  Coastal feature planting
-  Drainage channel
-  Stone seating
-  Kerb/edging
-  Mooring bollard
-  Lighting column

Please refer to drawing
no. 11632-LD-PLN- 002

Slipway

Existing parking for adjacent housing

~~Clifden Quay
housing~~

~~Quay~~
~~Warehouse~~

~~RNLI Lifeboat
Station~~

The Quay
House Hotel

Existing telescope retained

Existing Electricity
supply point retained

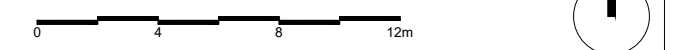
- Existing Electricity control panel retained

- Existing art panel and feature paving

retained

Equipment
Storage Area

-	23.09.22	First Issue	MW	LS
us	Date	Description	Dm	Chk



LUC

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Project
Clifden Town Centre Enhancements

Client
Galway County Council

Scale @A1

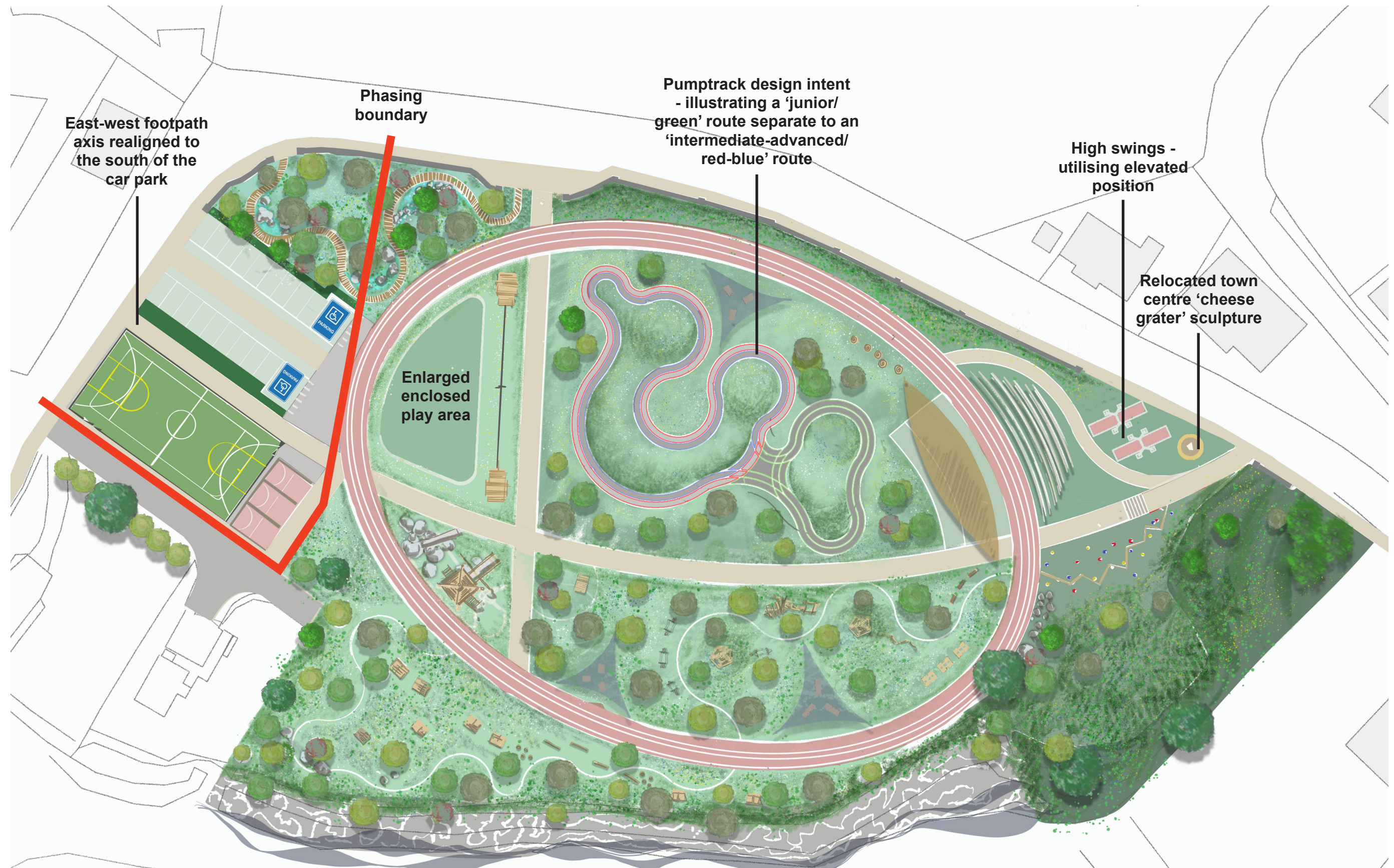
Status
FOR INFORMATION

Drawing Title
Beach Road Quay
General Arrangement

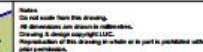
Job Nr 11632	Drawing Nr 11632-LD-PLN-003	Issue -
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Harbour Park

Masterplan concept with revised pump track layout, east-west footpath axis, additional entrance sculpture



Appendix B – Problem Map



- | | |
|---|---|
|  | Prepainted roller painting to hydrolysis |
|  | Prepainted self painting advanced surfaces |
|  | Prepainted self painting to carbonaceous corroding agents |
|  | Prepainted self painting to parking spaces |
|  | Prepainted asphalt surfacing to car damage |
|  | Prepainted self painting to carboniferous systems |
|  | Prepainted tactile surfacing |
|  | Prepainted concrete tactile painting to highway |
|  | Prepainted flush anti-skid optional for drainage |
|  | Prepainted rails with oxidation |
|  | Prepainted silver bonded rails to electrolysis |
|  | Prepainted plating |
|  | Prepainted wheel tire plating |

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www.lucdesign.co.uk

Project
Clifton Town Centre Enhancements

Client	Galway County Council
Project Name	
Project Number	
Project Manager	
Project Sponsor	
Project Start Date	
Project End Date	
Project Status	
Project Description	
Project Objectives	
Project Risks	
Project Deliverables	
Project Budget	
Project Resources	
Project Stakeholders	
Project Communication	
Project Reporting	
Project Review	
Project Approval	
Project Sign-off	
Project Closure	
Project Archiving	
Project Post-mortem	
Project Lessons Learned	
Project Feedback	
Project Evaluation	
Project Impact	
Project Sustainability	
Project Scalability	
Project Flexibility	
Project Adaptability	
Project Resilience	
Project Innovation	
Project Creativity	
Project Collaboration	
Project Teamwork	
Project Leadership	
Project Management	
Project Planning	
Project Execution	
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Project Leadership	
Project Management	

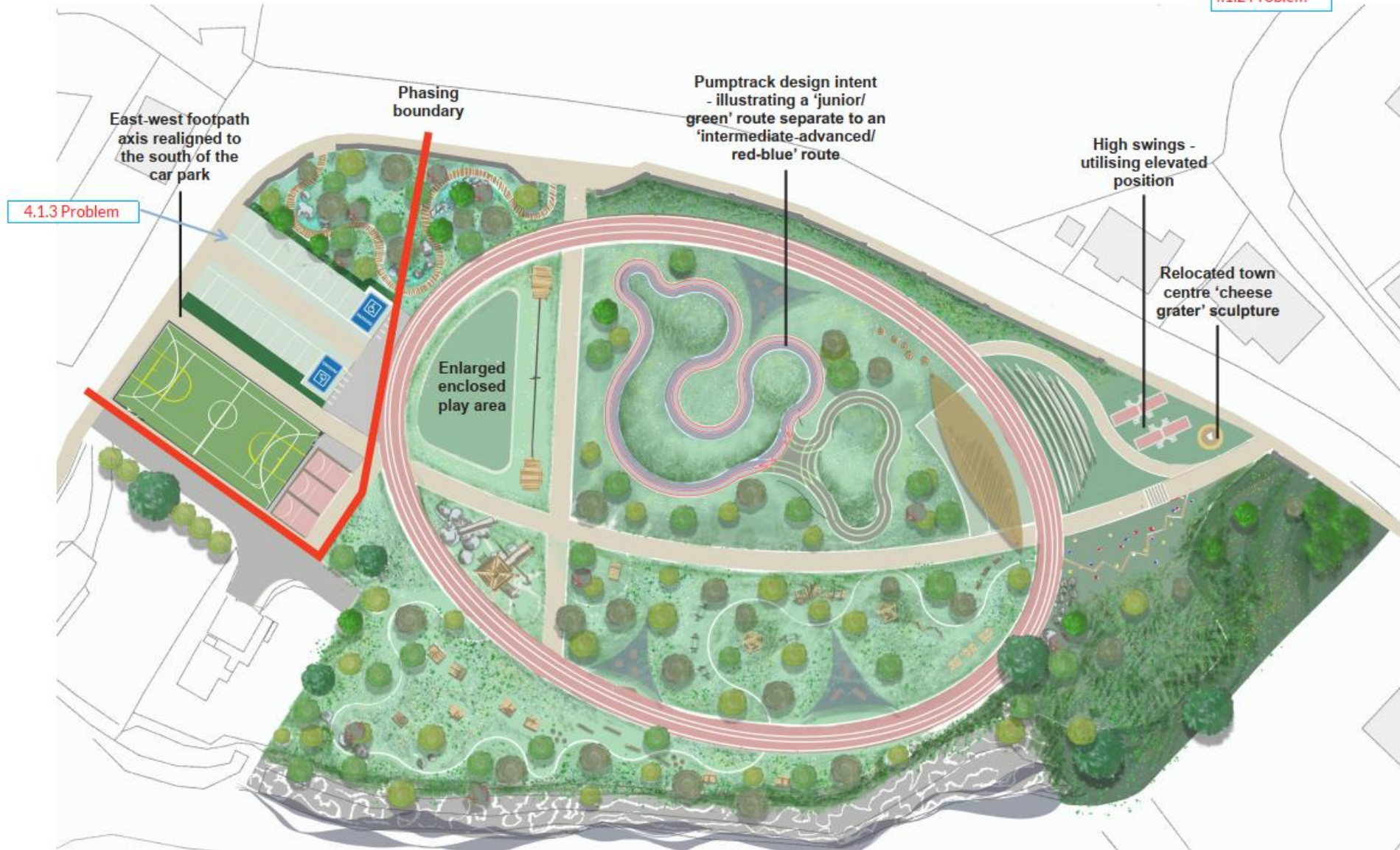
State ID#	State
1250	Information
Drawing Title	

Job to	Devolving to	to
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Harbour Park

Masterplan concept with revised pump track layout, east-west footpath axis, additional entrance sculpture

General Items
4.1.1 Problem
4.1.2 Problem



5.1.2 Problem

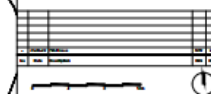
Please refer to drawing
no. 11632-LD-PLN-002

Notes:
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Existing and Potential

	Existing asphalt
	Existing quarry wall
	Existing sand stone point
	Existing feature parking
	Existing handrail
	Existing telegraph
	Existing utilities

-  Stone paving
-  Stone feature paving
-  Cassid feature paving
-  Drainage channel
-  Stone seating
-  Kiosk/bedging
-  Mossing island
-  Lighting column



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Project:
Clifton Town Centre Enhancement

Client
Galloway County Council

Scale @A1
1:500

Beach Road Quay

General Arrangement	
Job No.	Drawing No.

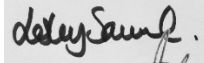
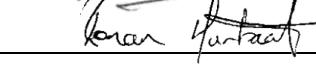
11632 11632-LD-PLN

Appendix C – Road Safety Audit Feedback Form

Road Safety Audit Feedback Form		
Scheme: Clifden Town Public Realm Enhancement Project Including Clifden Harbour Park and Beach Road Quay Clifden		
Audit Stage: 1	Route No.: R341	Date of Audit: 11/10/2023

To be Completed by Designer				To Be Completed by Audit Team Leader
Paragraph No. in Safety Audit Report	Problem accepted (yes/no)	Recommended Measures Accepted (yes/no)	Alternative Measures (describe). Give reason for not accepting recommended measure	Alternative Measures or reasons accepted by auditors(yes/no)
2.1.1	Yes	Yes		
2.1.2	Yes	Yes		
2.1.3	Yes	Yes		
2.1.4	Yes	Yes		
2.1.5	Yes	Yes		
2.1.6	Yes	Yes		
2.1.7	Yes	Yes		
2.1.8	Yes	Yes		
2.1.9	Yes	Yes		
2.1.10	Yes	Yes		
2.1.11	Yes	Yes		
2.1.12	Yes	Yes		
2.1.13	Yes	Yes		
2.1.14	Yes	Yes		
2.1.15	Yes	Yes		

To be Completed by Designer				To Be Completed by Audit Team Leader
Paragraph No. in Safety Audit Report	Problem accepted (yes/no)	Recommended Measures Accepted (yes/no)	Alternative Measures (describe). Give reason for not accepting recommended measure	Alternative Measures or reasons accepted by auditors(yes/no)
2.1.16	Yes	Yes		
2.1.17	Yes	Yes		
2.1.18	Yes	Yes		
2.1.19	Yes	Yes		
2.1.20	Yes	Yes		
2.1.21	Yes	Yes		
2.1.22	Yes	Yes		
4.1.1	Yes	Yes		
4.1.2	Yes	Yes		
4.1.3	Yes	Yes		
5.1.1	No	No	Kerbing and public lighting are shown on the LUC Beach Road Quay drawings. Tobin to provide required information on sightlines, swept paths, drainage, utilities.	Details of Kerbing / Lighting, Sight Lines, Swept Paths, Drainage, Utilities and Acc Works to be provided to the Audit Team at the Stage 2 RSA
5.1.2	Yes	Yes		

Signed:		Designer	Lesley Samuel	Date:	10.09.24
Signed:		Client	Galway County Council	Date:	15 October 2024
Signed:		Audit Team Leader	Ronan Murtagh	Date:	15/10/2024

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